

# Construction Management Plan

(including the Site Establishment Plan and Staging Plans)



## Document Control

Version	Action	Name	Date
1.0	Prepared by	Jack Bowyer	20/07/2022
	Reviewed by	Joel Yarham	20/07/2022
	Endorsed by	David Hill	20/07/2022
	Consented to		

## Amendment List

Amendment List			
Version	Action by	Changes	Date
1.0			

Amendments to this plan are to be undertaken consistent with the process outlined in:

- The Environmental Management Plan; and
- The Project Management Plan

## TABLE OF CONTENTS

<b>Executive Summary.....</b>	<b>5</b>
<b>Works Overview.....</b>	<b>6</b>
PROGRAMME .....	6
<b>Site Establishment Plan .....</b>	<b>8</b>
SITE AMENITIES .....	8
Site Deliveries & Materials Handling .....	9
<b>Construction Methodology .....</b>	<b>11</b>
Overview .....	11
Demolition & Preparation .....	13
Inground Service Work.....	17
Civil, Structure & Roof Awning Works .....	20
Site Working Hours .....	23
Work in Accessed Sites .....	23
Service Interruptions .....	23
Approach to Minimising Impacts .....	23
Site Induction .....	24
Document Management.....	24
Authority Approval Requirements .....	24
Construction Impacts .....	24
Emergency Procedures.....	24
Progress Reports .....	25
Site Cleaning.....	25
Site Security .....	25
Work Outside the Site .....	26
Signage.....	26

## Annexures

### **Annexure A**

Construction Programme

### **Annexure B**

Staging Plan

### **Annexure C**

Traffic Management Plan



## EXECUTIVE SUMMARY

This Construction Management Plan (CMP) describes the processes that will be applied, enforced and managed by the Construction Control (CC) team during the Newcastle Airport Terminal Expansion Enabling Works.

The processes and procedures identified and referred to within this CMP are required so that CC can meet its statutory and contractual obligations of the Project as well as maximise the safety, programme, quality, and financial outcomes. All works will be in accordance with the Work, Health and Safety Act 2011 and consolidated Regulations and the Construction Work and its Codes of Practice.

The control procedures for carrying out works under the CC Building Management System (BMS) is further described in the Work, Health and Safety Management Plan and our Employee Relations Management Plan.

CC will maintain a “whole of project risk assessment” that breaks up bodies of works into steps, identifying and listing all potential hazards/risk and controls.

For each element of works within the Project, this CMP provides an outline of CC’s approach to each of the following key elements:

1. Overview of construction methodology & programme
2. Pre-construction planning
3. Construction scope
4. Contractor management
5. Site Specific Overview
6. Cleaning & Rubbish Management
7. Management of Construction Impacts
8. Site Security
9. Works outside the site
10. Preliminaries Plant & Equipment

During the construction this plan will undergo continuous amendment as subcontractors are appointed, project risks fully captured, and procedures further developed and finessed.

## WORKS OVERVIEW

### PROGRAMME

#### GENERAL

To facilitate the expansion of the Terminal Building, the existing Premium Carpark must be relocated to the north-west into the existing Short Stay Carpark (adjacent to the existing Airport Terminal).

The Construction Programme attached has been prepared on *Microsoft Project 2010* and is displayed in a linked bar (Gantt) chart format.

The programme indicates interdependencies between activities that accurately represent the methods for executing the whole of the works. It includes provision for the off-site fabrication, manufacture, and procurement of all major plant & equipment.

The programme indicates the sequence of work, periods which various phases of the works are to be executed, allowances for holidays, signification milestones, activity inter-relationships, including those activities to be undertaken by subcontractors and suppliers, both on and off-site.

In addition, the programme is in accordance with the requirements as per the Contract Tender Schedules issued by NAPL.

The onsite working time is based on a five-day working week and includes non-working day allowances for Fixed Rostered Days Off (RDO's) and public holidays. The off-site working time has been based on a five-day working week and includes non-working day allowances for public holidays.

The project is scheduled to start on site September 2022 and be completed by April 2023. The overall gross construction period from commencement to completion of all works is 40 Calendar weeks. This includes a nominal allowance for inclement weather delays.

Our programme provides for substantial completion & handover of the new Premium Carpark, the energisation of the Solar Panels and activation of the Electric Vehicle Chargers may not be commission in its entirety. The energisation of the works is based on the proposed substation upgrade, as this is yet to be designed CCs are unable to confirm a date of completion.

The programme is presented in Annexure A.

#### OFF-SITE WORKS

Construction Control has identified key off-site works which it will manage and monitor to ensure that lead-times have minimal adverse impact on the construction programme, these include trade related items & Client Deliverables.

CC has noted the first working day is a successor for NAPL obtaining a Construction Certificate which is linked to the acceptance of a Development Approval. Both tasks are the Client Deliverables & can affect the date of commencement on site.

The completion of the new Short Stay Carpark #2 and emptying of the existing carpark are preconstruction conditions.

Construction Control has identified key trade elements which have extended lead times:

- Structural Steel
- Solar Panels which include invertors & other associated equipment
- Electrical
- Civil Drainage materials
- Cladding materials

To minimise risk CC will seek early design and engagement of subcontractors.

## SITE ESTABLISHMENT PLAN

### SITE AMENITIES

### STAFF AND WORKER AMENITIES

The allocated area for Construction Control's Site Office is located on the Northeast Corner of the proposed work zone. The area will be utilised by CC up until the Site Compound for the Terminal Expansion (Main Works) Project is established.

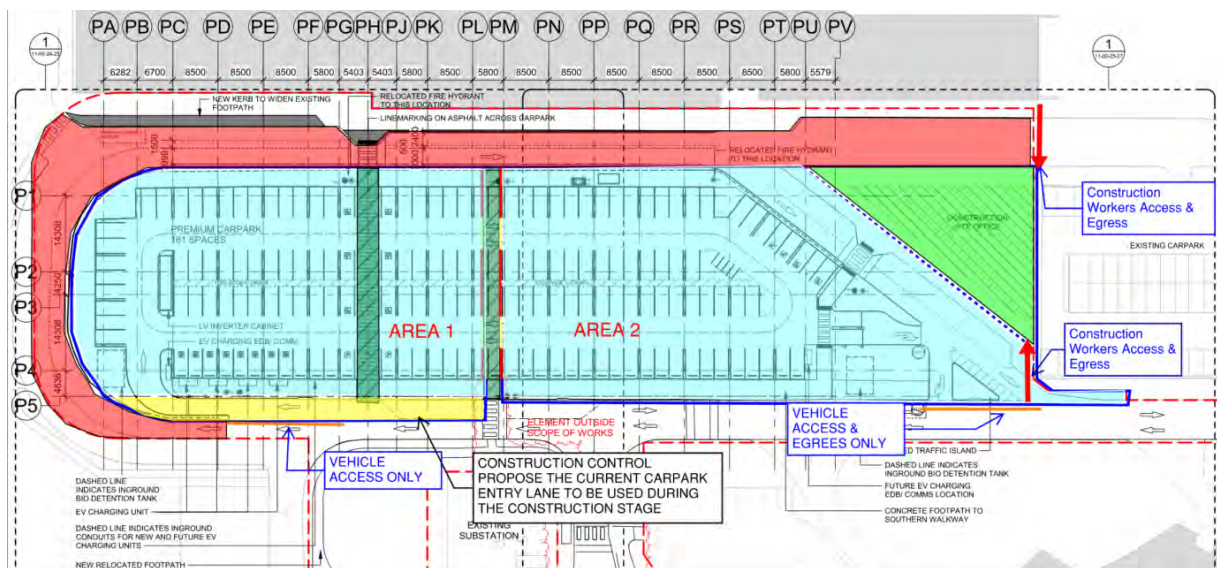


Figure 1 – High Level Site Plan

See below the proposed layout which will include:

- 1 of 12m x 3m Office with Kitchenette
- 1 of 12m x 3m Lunch Room
- 1 of 12m x 3m Changeroom
- 1 of 6m x 3m Meeting Room
- 1 of 6m x 3m Toilet Blocks

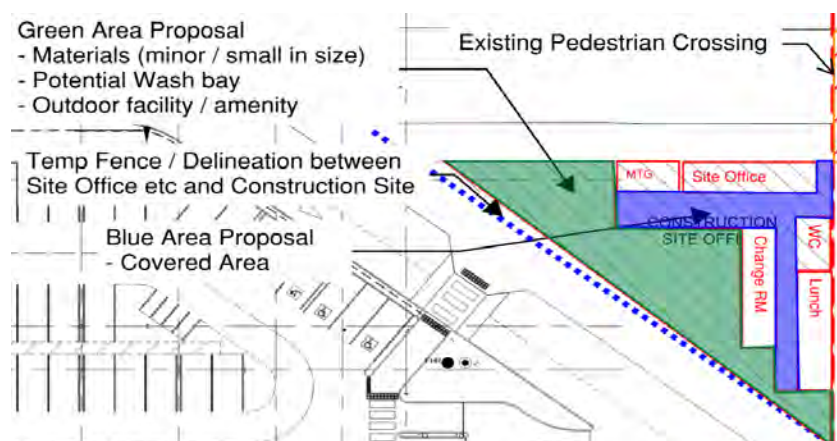


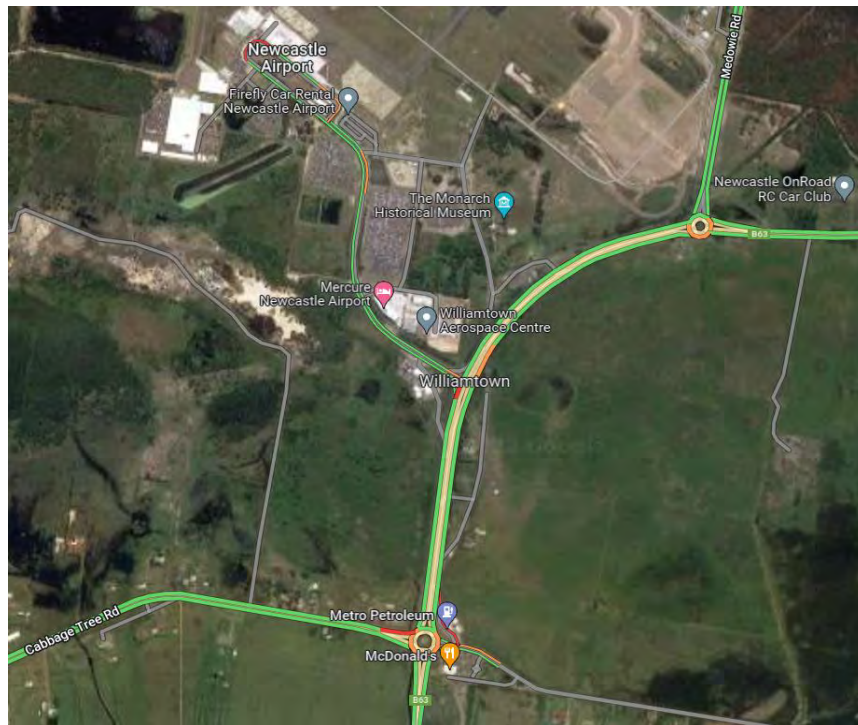
Figure 2 – Site Office/Compound Layout



## Site Deliveries & Materials Handling

Newcastle Airport is in Williamtown NSW, 2318, with a Street Address of 1 Williamtown Drive. The airport is approximately 25kms from the Newcastle City & is off Nelson Bay Road.

Site Deliveries will use either Nelson Bay Road or Cabbage Tree Road as the majority of industrial suburbs and highways will link to these two main roads.



*Figure 3 – Google Aerial of the Newcastle Airport*

CC will work with Airport Operations to adjust delivery times for Departures & Arrivals and recommend that a weekly schedule is issued which details flight times for both departures and arrivals, the use of a schedule will help CC confirm delivery times.

CC will manage the impacts of construction deliveries using Veyor. Veyor is a live planning tool for the construction industry, connecting all parties in real time to improve field coordination and efficiency. Subcontractors will need to book by requesting a day and time which requires the Site Manager to accept. All users/members of the system can check week(s) ahead and plan accordingly.



## TRAFFIC MANAGEMENT

Construction Control has proposed the below Traffic Management Methodology. The aim is to provide a strategy for the management of plant and people within the Airport Precinct.

The Traffic Management Plan has been developed to best manage the following:

- Airport vehicle & foot traffic
- NAPL staff & deliveries
- Construction vehicle & foot traffic

Deliveries during normal hours will enter & exit via the South Boundary Lane, there will be a traffic controller managing the construction gates along the South Boundary. Traffic Controllers will have the following qualifications:

- National OHS Construction Induction Training (whitecard)
- RMS Traffic Controller Photo card
- RMS Implement Traffic Control Plans Photo card
- Comply with the direction of authorized site personnel on site

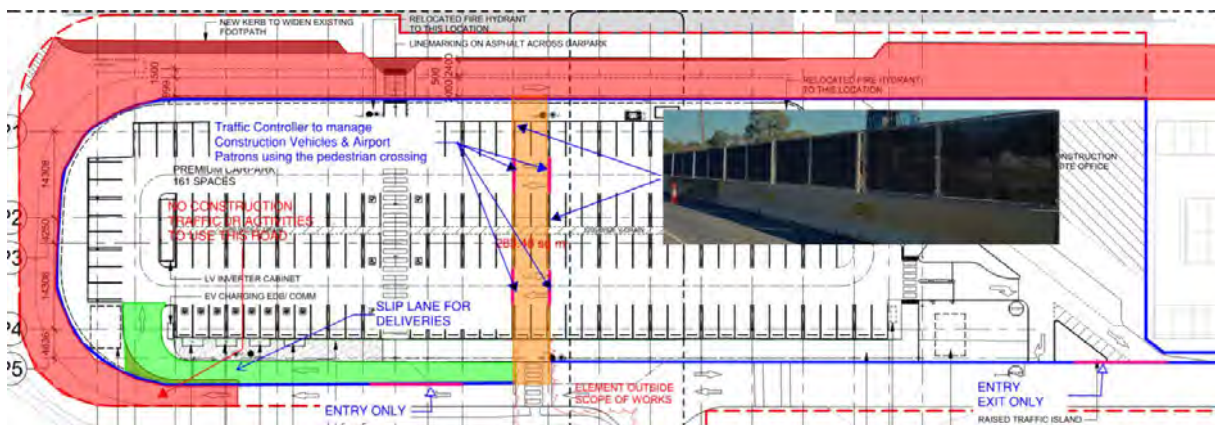


Figure 4 – Construction Site & Materials Handling Plan

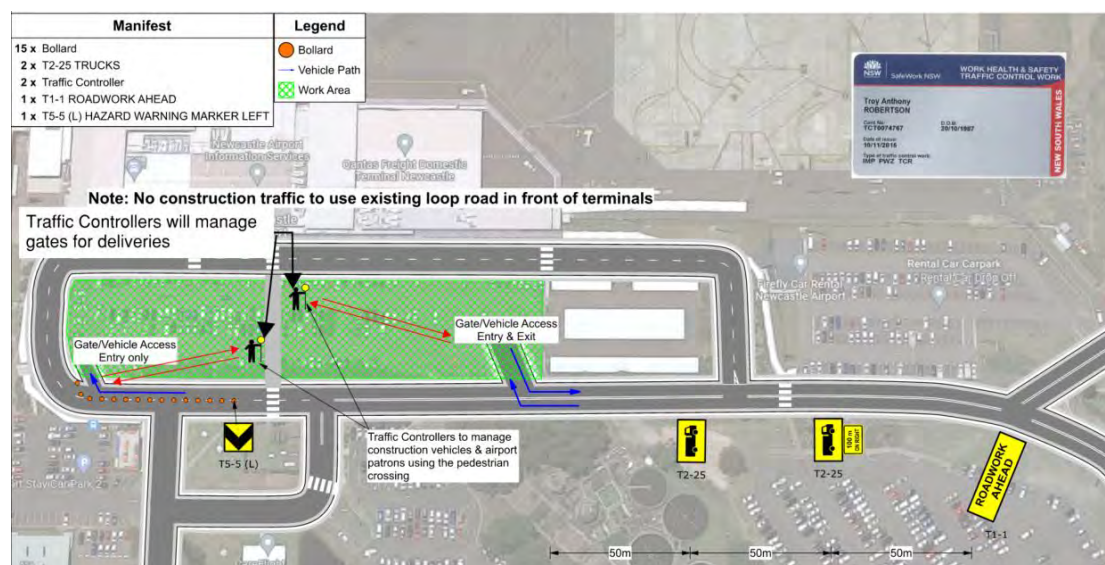


Figure 5 – Proposed Traffic Management Plan



## CONSTRUCTION METHODOLOGY

### Overview

The purpose of this section of the Construction Management Plan is to outline the construction planning that has been developed during Early Contractor Involvement (ECI) Period. This planning provides the foundation for the Construction Programme & methodology proposed.

It provides an opportunity for CC to:

- provide the reader with a concise outline of our approach to commencing and completing the works.

Majority of the works take place within the site perimeter, works which are conducted outside the main construction zone will be completed out of hours & agreed in consultation with NAPL Operations Team to ensure there are no impacts.

CC's programme breaks the projects scope into elements & areas in which the works are completed. The proposed methodology will:

- Assist with the coordination with NAPL Operations
- Provide a positive experience for Airport Users
- Provides versatility for the completion of works outside the main construction zone

CC's proposed site plan ensures that there will be minimal impact to Airport Operations, Public protection & construction efficiency (deliveries & materials handling).

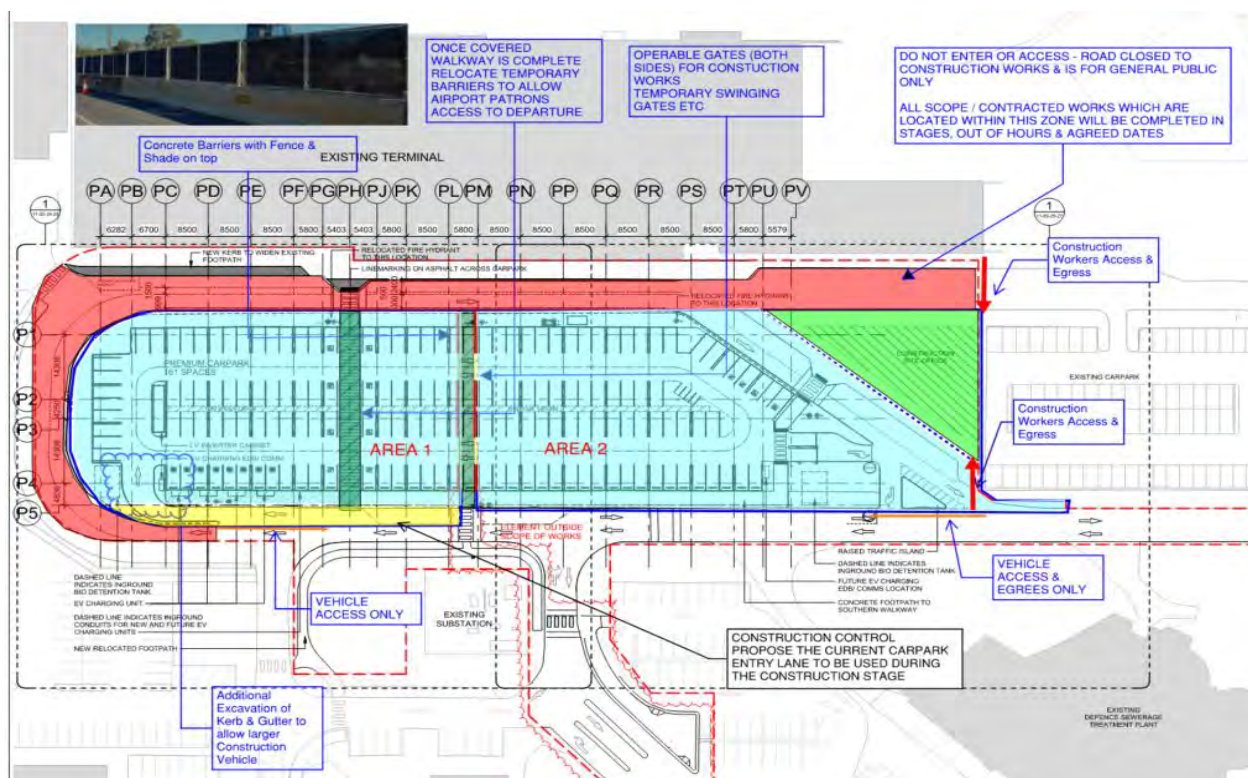


Figure 6 – Detailed Site Layout

Construction Control appreciates the need to ensure Airport Patrons have a positive experience whilst construction work is underway, including the recognition and respect of timing and minimal impact to Airport Operations.

CC have identified the current loop road (parallel to the terminal) as a Permanent Exclusion Zone during *normal working* hours. Works & deliveries which require access will be completed out of hours which will include consultation & approval with NAPL's Operation Team. See the Figure 1 of CCs proposed Site Plan (highlighted in Red).

### **Airport Users**

CC will maintain the existing Pedestrian Access (existing carpark) through site to ensure safe passage to airport terminal.

The image below illustrates the use of concrete barriers with fence screens, to separate pedestrian access from construction activities. The protection will be maintained during construction until such time the new covered walkway is complete (located on Grid PH).



*Figure 7 – Image of a F Type Barrier with Fence Panels*

### **Airport Operations**

Construction Control planning and programming has aimed to ensure minimal to no impact is caused to Airport Operations. The programme has taken into consideration all works which need to be completed out of the normal working hours and the appropriate allowances to have the works completed in timely manner.



## Demolition & Preparation

### AREA 1 – MAIN CARPARK AREA

The works included in the existing carpark will have minimal to no impact to the airport's daily operation.

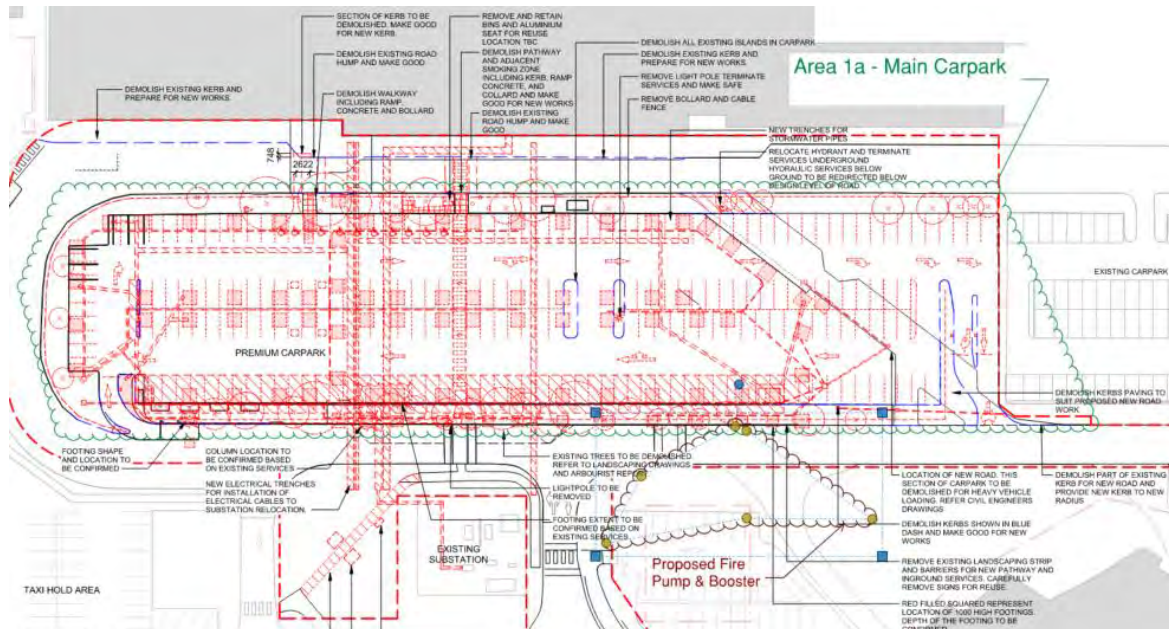


Figure 8 – Area 1a – Main Carpark Scope Area

#### Scope of Works:

- Service Isolation including removal of redundant services
- External perimeter trees
  - o All machinery & equipment will be stationed within the site compound
  - o Trees will be cut at low level & grinded below ground level
- Existing kerbs & Islands
  - o Use of Excavators with drill hammers and buckets
- Ground Preparation for New Loop Road Connection



Figure 9 – Image of Stump Grinder

### AREA 1B – RELOCATE FIRE HYDRANT NORTH BOUNDARY

The proposed New Loop Road requires the isolation, disconnection & relocation of a single Fire Hydrant. To enable the civil works to commence the following scope is proposed to be completed within the construction site.

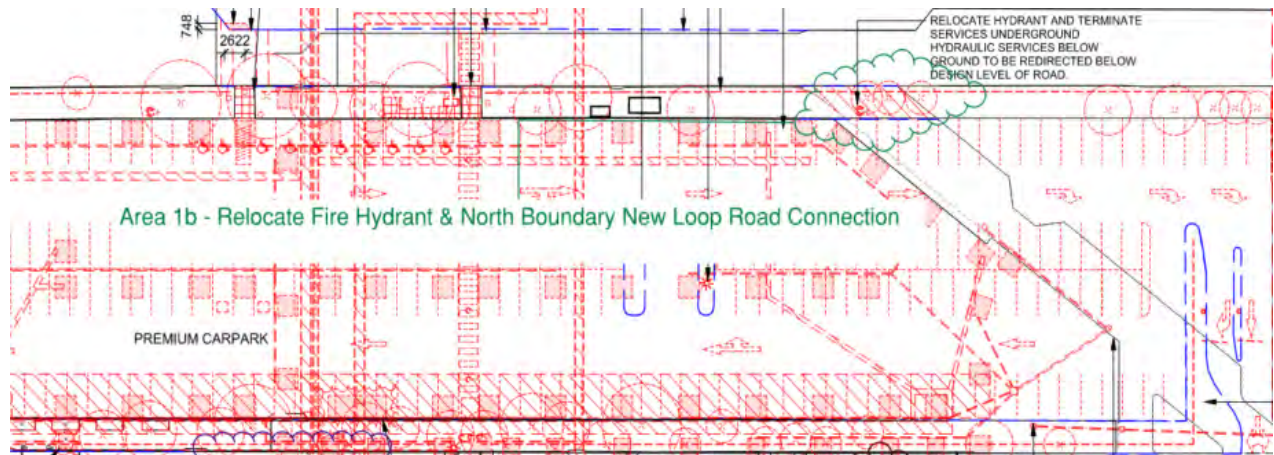


Figure 10 – Area 1b Relocation of Hydrant

#### Scope of Works:

- Isolate Main
- Excavate (use of hydrovac due to the potential issues with existing services)
- Re install Hydrant
- Test & Commission
- Remove Existing Kerb & Gutter

Majority of works should occur during normal hours, the isolation of exiting & connection of relocated hydrant may occur after hours. Construction Control aim to ensure that the works do not impose any impacts to the Terminals Fire Hydrant System, all required impairments will be completed in line with NAPL requirements. Further investigation will be needed to establish current main and its service to the Terminal.

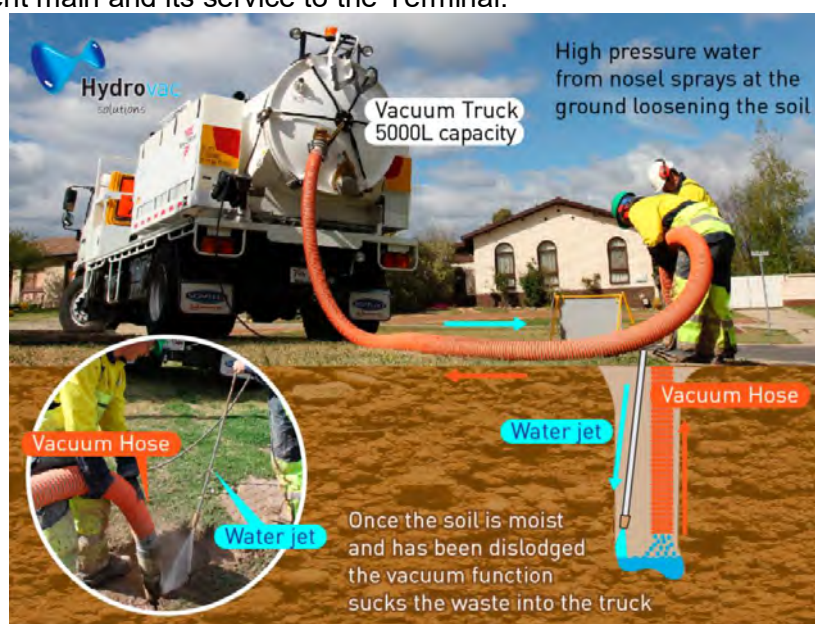


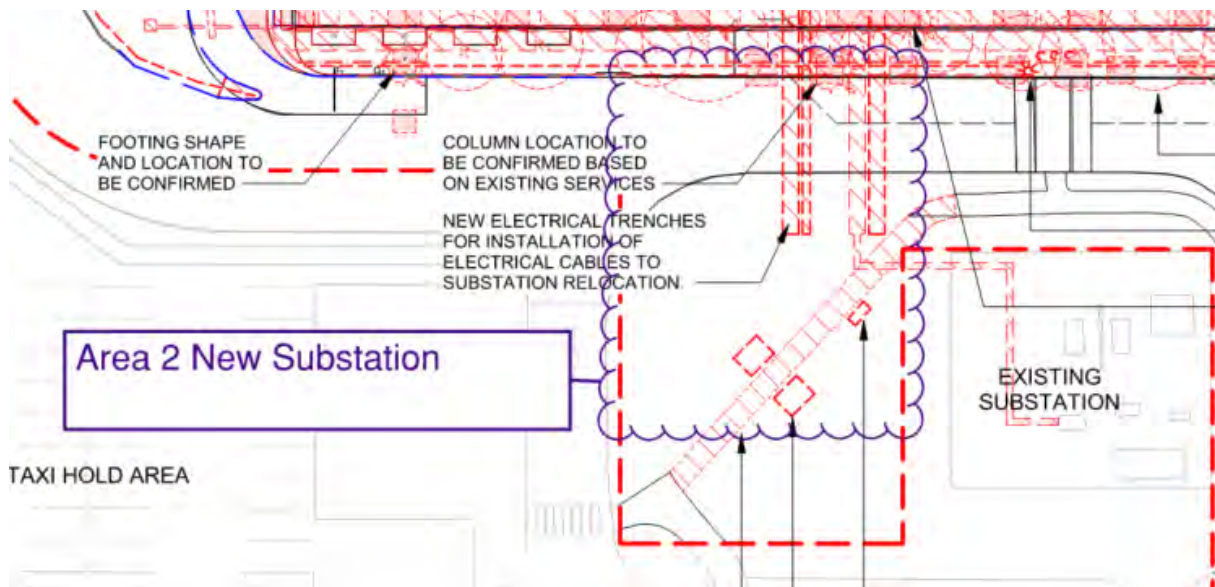
Figure 11 – Image of the use of a Hydovac



## AREA 2 NEW SUBSTATION ENABLING WORKS

As a result of the location of works, it will need to be completed after hours to ensure no impact. Temporary road plates will need to be used until the works are complete.

The work includes the installation of High (HV) & Low (LV) Voltage conduits to the proposed new substation location.



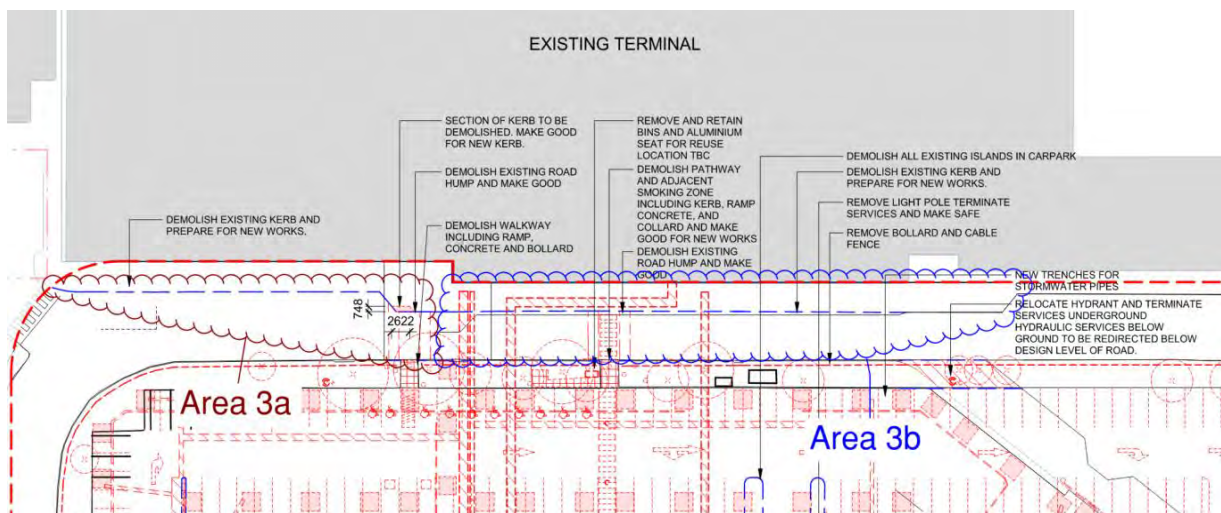
*Figure 12 – Area 2 Proposed New Substation Location*

### Scope of Work:

- Excavation of exiting road (using either or both an excavator and hydrovac)
- Install of conduits LV, HV, Communications & Solar
- Installation of pits for LV, HV, Communications & Solar

### EXISTING TERMINAL FORECOURT (AREA 3A & 3B)

Construction Control recognises that the works (identified as 3a & 3b) will need to be primarily completed out of hours, CC will further develop the construction methodology in consultation with NAPL to ensure public safety & minimise operational disturbances.



*Figure 13 – Areas 3a & 3b, Existing Terminal Forecourt (Departure & Arrivals)*

#### Scope of Works:

To limit airport disruption the following works will occur after hours.

##### Area 3a

- Demolition of Existing Kerb
- Set Out, Form & Pour Kerb
- Temporary pavement for future reinstatement
- Remove Existing Road Hump & Make Good

The following works will occur both during normal working and after hours, CC will coordinate with NAPL to agree on when works are conducted.

##### Area 3b

- Demolition of Existing Kerb
- Excavation of New Electrical Trench for Conduits & Cables
- Proposed New Road Crossing
- Set Out, Form & Pour New Pedestrian Ramp & Kerb
- Linemarking



## Inground Service Work

Construction Control has identified inground services will occur both within & outside the Construction Site.

The three main services identified are Hydraulic including Stormwater, Electrical & the New Fire Hydrant Ring Main.

Works to be complete outside the construction site will occur after hours and require consultation with NAPLs Operations Team.

## HYDRAULIC & STORMWATER

The hydraulic services connections points for the Premium Carpark will be provided as follows:

- Potable Cold Water – System extends to carpark to supply water for landscape hose taps and also fire hose reels.
- Rainwater Drainage – From proposed Civil stormwater inground system taken from/to each downpipe connection. The rainwater system captures all awnings over the carparking bays as well as the walkways.

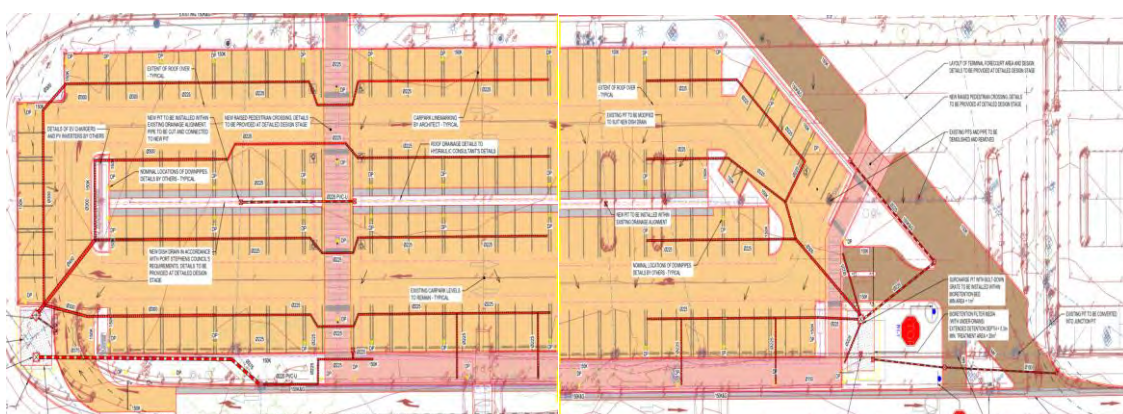


Figure 14 – Current Stormwater Layout

### Scope of Works:

- Excavation
- Pits and Spoon Drain
- Bio Retention
- Install of Pipes, T Junctions, and Isolation caps
- Inspection, Backfill and Compaction
- Roof Downpipes & the connection into Stormwater
- Test & Commission

Prior to excavation works existing services will be identified through, existing as built, Dial Before You Digs (DBYD), & Service Identification.

At identified high risk locations Hydrovac will be utilised to locate exact positioning.

## ELECTRICAL

Included in the Electrical trade package is High & Low Voltage including communications. The Power for the Solar Panels are included in the following section. This section of works will occur during the demilition stage.

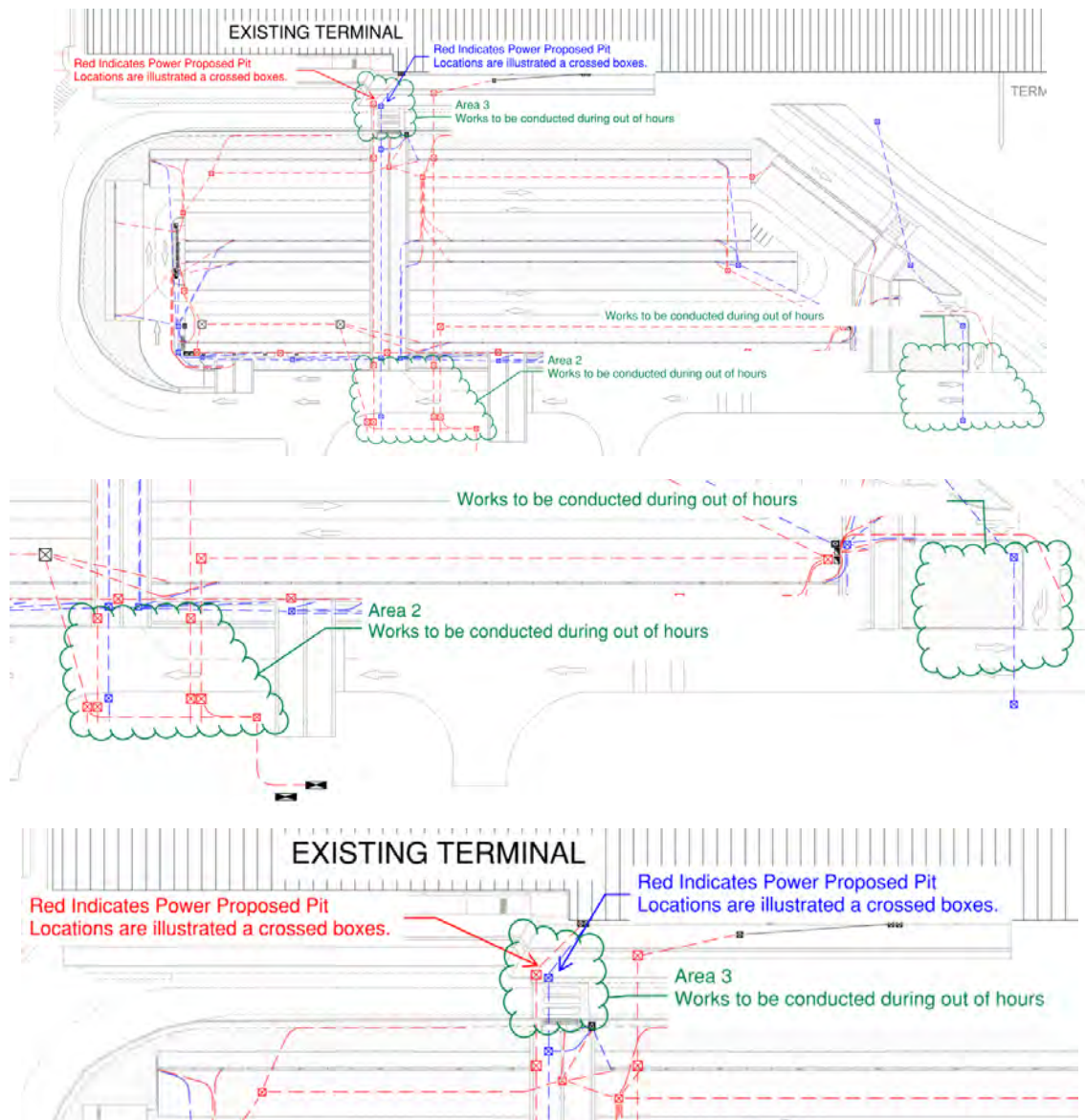


Figure 15 – Electrical (HV, LV & Communications)

### Scope of Works:

- Excavation
- Install of Pits and Conduits
- Cabling & Termination
- Fill, Compact & Make Good to existing surface (if outside construction zone)

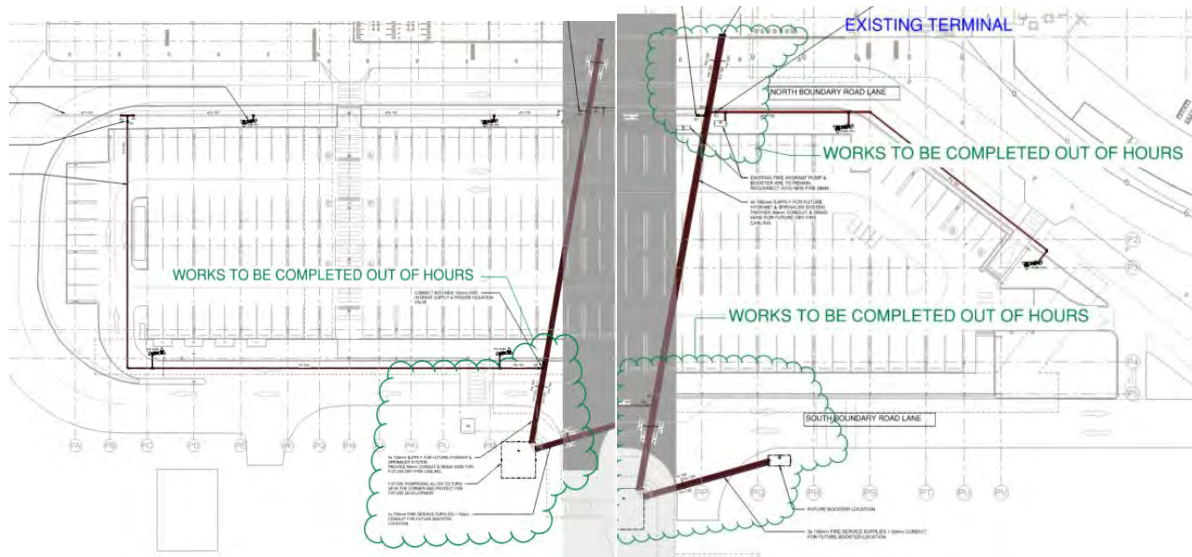
Prior to excavation works existing services will be identified through, existing as built, Dial Before You Digs (DBYD), & Service Identification.

At identified high risk locations Hydrovac will be utilised to locate exact positioning

## FIRE HYDRANT

The proposed works will upgrade the current Fire Hydrant Main.

A proportion of the Fire Hydrant works will need to be completed during non-normal working hours and in stages with coordination & approvals from NAPLs operational team. As per the below screenshots the scope will provide essential fire services to the existing terminal which is future provision for the terminal expansion.



*Figure 16 – Fire Hydrant Ring Main*

### Scope of Work:

- Excavation & Trenching
- Installation of hydrant pipes including conduit for Dry Fire (electrical cables)
- Provisional / Preparation work for proposed locations for Pumproom & Booster
- New Fire Hydrant Reels & Hoses
- Isolation Valves

Prior to excavation works existing services will be identified through, existing as built, Dial Before You Digs (DBYD), & Service Identification.

At identified high risk locations Hydrovac will be utilised to locate exact positioning



## Civil, Structure & Roof Awning Works

### NEW LOOP ROAD CONNECTION

The proposed New Loop Road as well as upgrading the existing carpark is the predecessor for the next stage of construction for the terminal expansion. Construction Control have identified the works as a major focus. Following the relocation of the existing Fire Hydrant and ground preparation works the new road will commence.

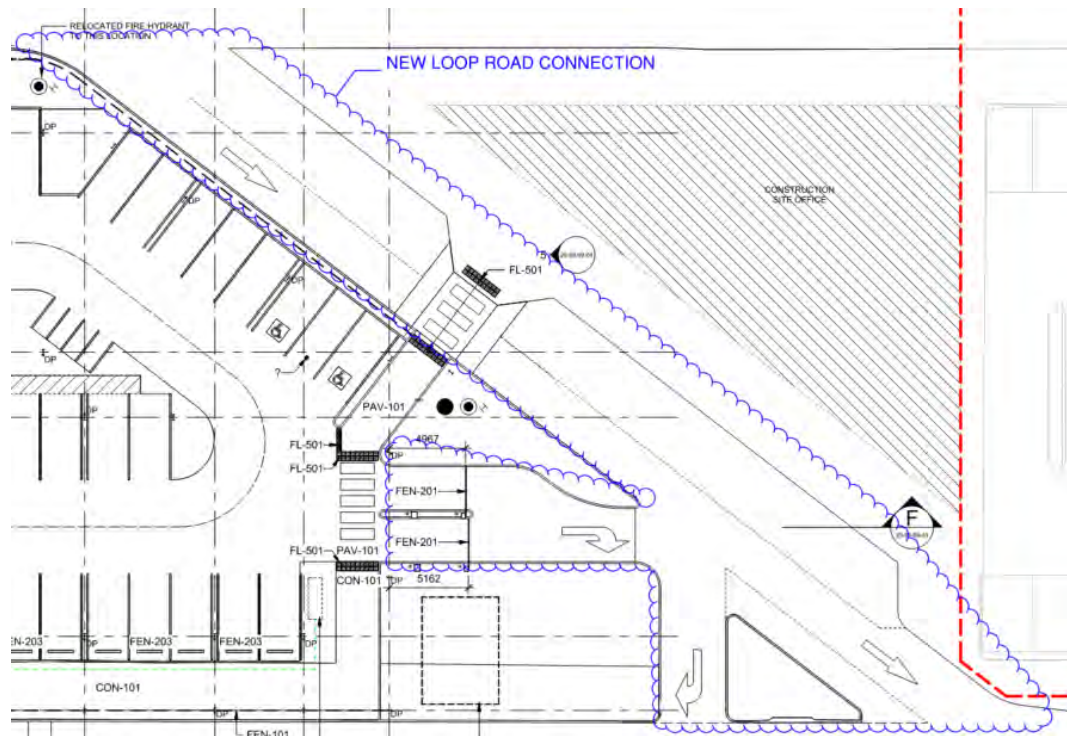


Figure 17 – New Loop Road

### Scope of Works

- Survey & Setout
- Cut & Trim Road Layout
- Inground Works
- Kerb, Gutter & Ramps
- Compact & Asphalt



## AREAS/STAGES 1 & 2

To allow Airport Users access from carparking and to ensure there is construction continuity the site is split into two (2) areas. The sequencing of the carpark works will progress from West to East.

To ensure there is continuity subcontractors will provide multiple crews to complete the works.

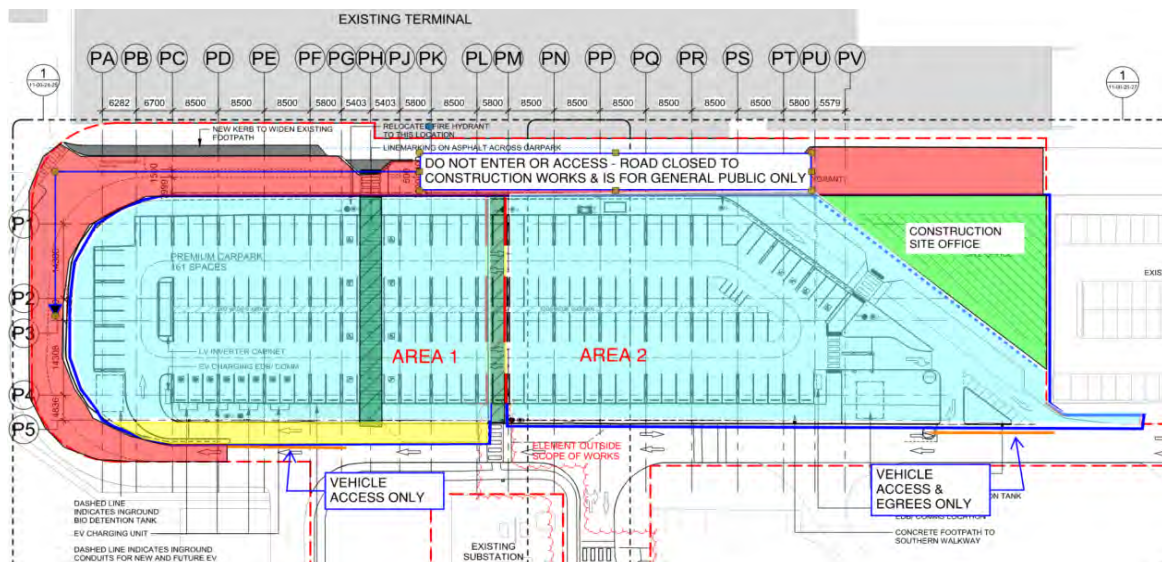


Figure 18 – New Loop Road

### Scope of Works:

- Civil including the excavation and pour of pad footings, kerb, gutters, ramps & asphalt
- Steel Columns, Beams, Rafters & Purlins
- Remill & Asphalt carpark
- Solar Panels
- High Level Services
- Roofing including gutters and downpipes
- Cladding

The Structural Steel works will be installed using a Franna and appropriate size scissors.

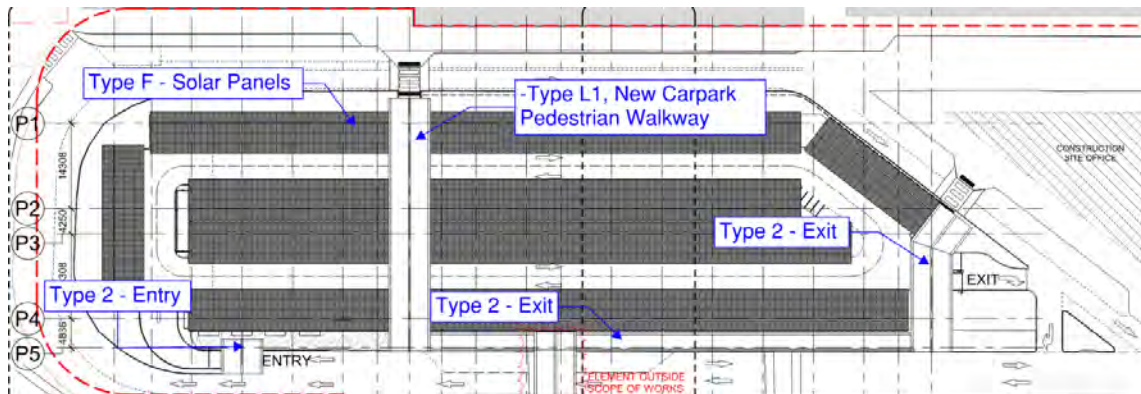


Figure 19 – Franna & Scissor (All Terrain)

## COVERED WALKWAYS & AWNINGS

The new premium carpark will consist of carpark bays with solar panel awnings, and covered walkways.

See below the overall layout of the awnings & the different types.



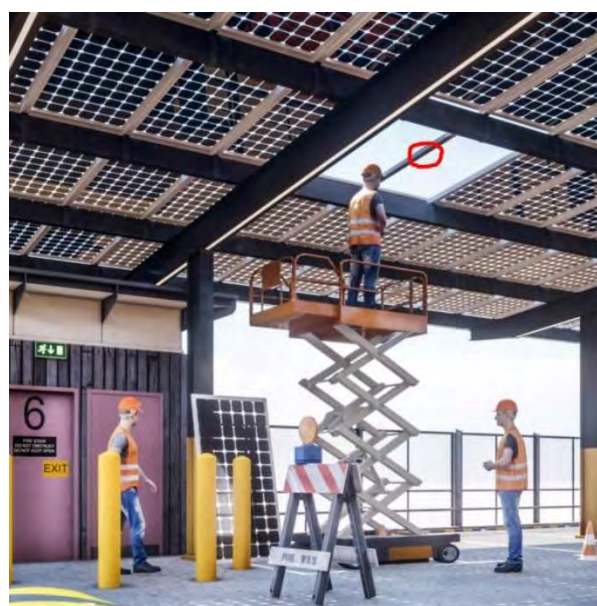
*Figure 20 – Walkway & Awning References*

Note that the works outside the main carpark/construction zone has not yet been allowed.

Scope of Works:

- Install of Columns & Beams
- Services (Electrical, Security & Hydraulics)
- Roof Sheeting including Gutters
- Soffit Cladding
- Fascia & Flashings
- Roof Safety & Access

The install of Solar Panels will be installed from Ground Level (bottom-up approach). The use of scissors (see image below) will be utilised to install the solar panels.



*Figure 21 – Example of Solar Panels being Installed*

## Site Working Hours

Construction Control are proposing no restriction on working hours during construction.

Works to be conducted outside the main construction site requires consultation with NAPL, work periods to be agreed.

Construction Control does appreciate that the Airport will facilitate flights throughout the day and there will be peaks & lows with departures and arrivals. The project team will work with an airport schedule i.e., weekly to limit operation impacts.

## Work in Accessed Sites

- CC shall ensure all works in 'Accessed Sites' are coordinated between the Project Manager (Joey Yarham), Site Manager (Dean Sweeney) or Project Engineer (Jack Bowyer) as well as Newcastle Airport Pty Limited (NAPL) Senior Project Manager (Josh Fullerton) where necessary, to ensure minimal disruptions to public and/or Airport operations.
- Accessed sites will be defined by temporary fencing around the perimeter with jersey kerb/fence barricade along walkway which will be open for pedestrian access during construction. The temporary fencing can be utilised by NAPL for marketing, informational signage, and/or wayfinding as required.

## Service Interruptions

Construction Control will obtain relevant DBYD & survey reports before the commencement of any ground penetration works. This will include:

- Tree Removal
- Fire Hydrant Relocation
- Demolition of existing surfaces and services

Construction Control do not currently forecast any major services interruptions during the works. The proposed upgrade of the existing Fire Main may temporarily affect however the use of isolation valves and alternative sources will be investigated.

Construction Control will notify NAPL & their Operations team if there any service interruptions. Notification will be issued across for review and approval generally with a minimum of one (1) weeks' advance notice.

## Approach to Minimising Impacts

CC shall ensure all workers complete appropriate inductions and that site security procedures are known by applicable personnel. CC shall ensure all access / egress into the site is maintained closed and no unauthorised persons are within areas at any time.

Our Site Managers are responsible for ensuring the following:

1. that all contractors entering the site have been site inducted.
2. that all persons entering the site are wearing appropriate PPE.
3. that a coordinated interface exists between airport operations, construction movements and general deliveries.



4. that any shared road, access & egress is always kept clean; and
5. that construction vehicles entering are not parking for extended periods after dropping off materials.
6. the existing and loop road is not used by any construction traffic

Construction Control propose the use of the current turn in lane for the carpark as our slip lane (Green Zone). The current use of the lane is for the carpark only & will have no impact on Airport Traffic.

### Site Induction

All subcontractors will be required to complete a Construction Control standard Site Induction, which will include NAPL requirements.

### Document Management

The project will use Aconex for Document Management.

Aconex is an electronic document management system to serve comprehensive tools for construction businesses looking for project delivery, productivity, efficiency, and management.

Aconex helps improve project management by centralising communications, whether it be email-like communications or customized forms to match the unique business processes involved in a project.

### Authority Approval Requirements

This is addressed in the Key Stakeholder Management Plan for the Project.

### Construction Impacts

Site Establishment and Control plans & procedures will be detailed to manage each source of adverse impact – dust, noise, vibration etc. Construction Control shall ensure minimal impact on client and public activities during construction works by ensuring all works are completed in accordance with legislative requirements and these agreed plans.

Where feasible Construction Control shall ensure works are isolated to mitigate exposure to outside activities.

In addition, the Risk Assessment prepared for the Project contains proven and effective treatments that will be used to control environmental risks.

### Emergency Procedures

The proposed emergency procedures for the Project are addressed below. Additional materials governing the general approach to management of emergency on site is addressed in the WHS Management Plan and the Risk Assessment prepared for the Project.

CC will develop and maintain a Project Emergency Plan (PEP) in consultation with stakeholders to manage potential construction, client and public activities; and be communicated to all at site induction. The PEP shall incorporate client emergency



procedures for the Airport and surrounding works and be reviewed to maintain appropriate to the changing environment.

CC will at all times maintain access/egress to emergency provisions such as fire fighting equipment, first-aid arrangements and emergency services.

Access/egress to work areas, interface with public / client activities and specific emergency provisions are inspected and reviewed formally by the Site Foremen/Managers during weekly project HSE Inspections.

## Progress Reports

The approach to reporting is addressed in the Communications Management Plan and the Project Administration Management Plan for the Project.

## Site Cleaning

### PROVISION FOR SITE BINS

Rubbish bins will be provided in various locations onsite. The Civil/Demolition contractor will be responsible for the removal of excavated material from the carpark works and will provide a bin or stockpile on site for removal off site.

### PROHIBITION ON USING AIRPORT WASTE BINS

No NAPL waste bins will be used.

### RECYCLING TARGETS

The recycling target is addressed in the Environmental Management Plan for the project.

### SOURCE SEPARATION

Provision will be made for:

- General waste
- Construction waste
- Co-mingled recycling

The Demolition & Waste Management contractor will be required to provide evidence of recycling targets.

## Site Security

### PREVENTION OF UNAUTHORISED ENTRY

CC shall ensure all workers complete appropriate inductions and that site security procedures are known by applicable personnel. Construction Control shall ensure all access / egress into the site is maintained closed and no unauthorised persons are within areas at any time.

At the main pedestrian site entry point a turnstile gate will manage access and only those who have been site inducted will be granted access.

## CREDENTIALS

Our Site Manager is responsible for ensuring the following

1. All contractors entering the site have been site inducted
2. Ensuring all persons entering the site are wearing appropriate PPE
3. Ensuring that a coordinated interface between construction movements and general deliveries
4. Ensuring that the dock is kept clean at all times and
5. Ensure that construction vehicles entering are not parking for extended periods apart from to drop off materials etc.

## Work Outside the Site

### OBTAINING APPROVALS

Work outside the site shall be isolated from pedestrians and public activities with appropriate fencing/hoardings and signage fixed as necessary to warn and direct people away from construction activities.

Disruptive Access Notice (DAN) will be the mechanism for NAPL works approval outside of the site confines.

## Signage

### PROJECT SIGNBOARD

Construction Control will maintain project signboard at multiple locations around the site perimeter. This will provide all statutory information.

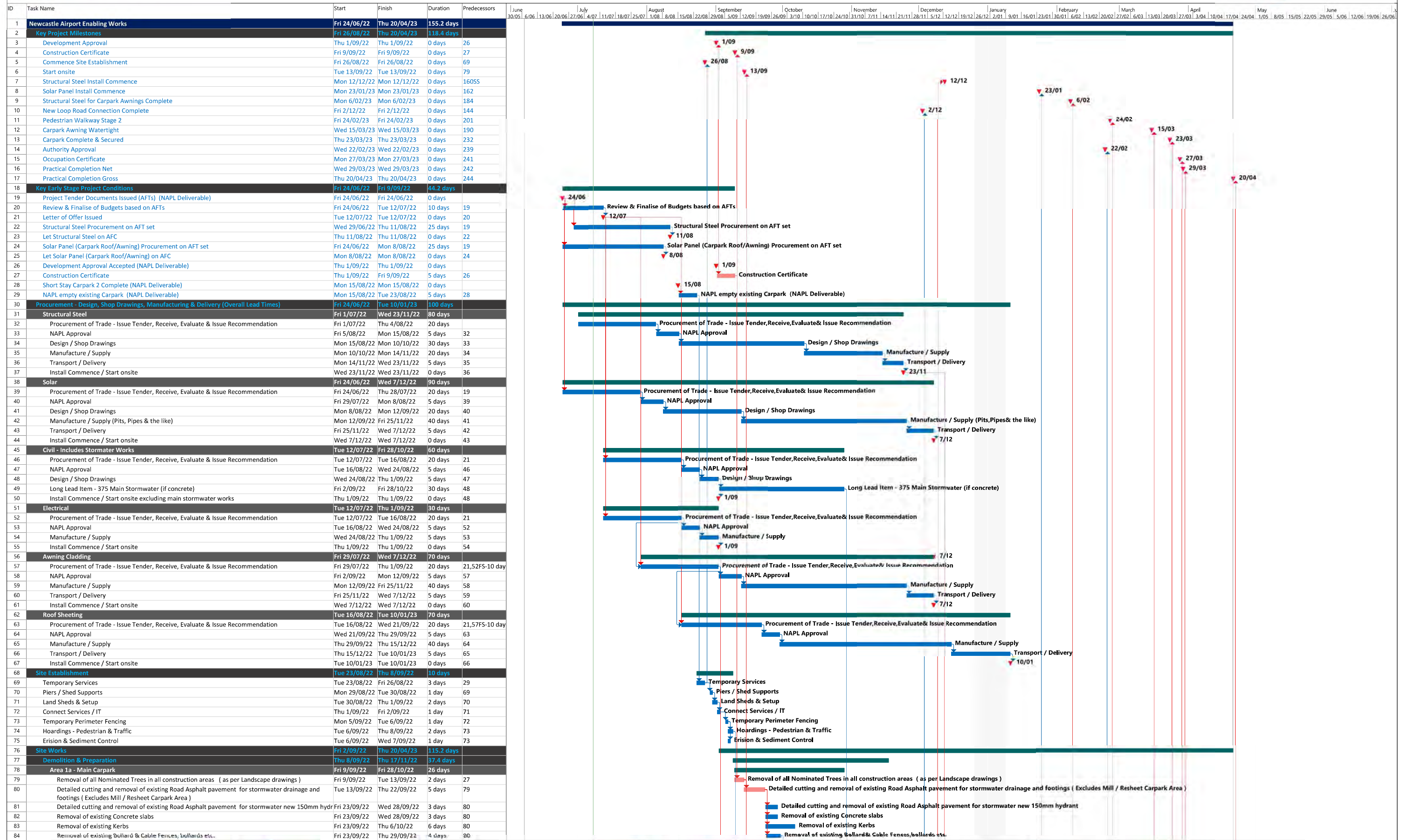
### STATUTORY AND SAFETY SIGNAGE

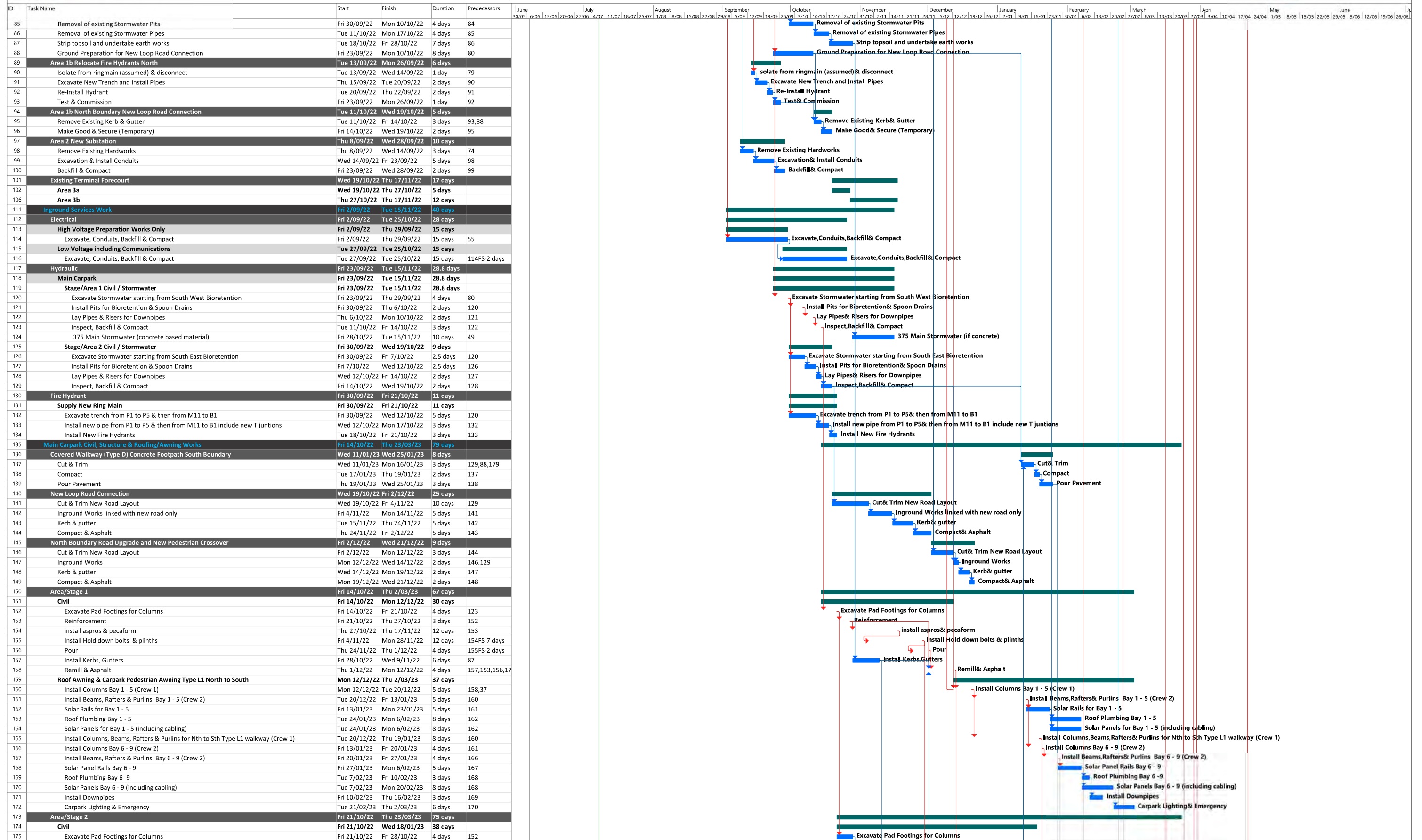
Where appropriate, existing NAPL signage will remain and additional safety signage will be used as required.

# Annexure A

## Construction Programme





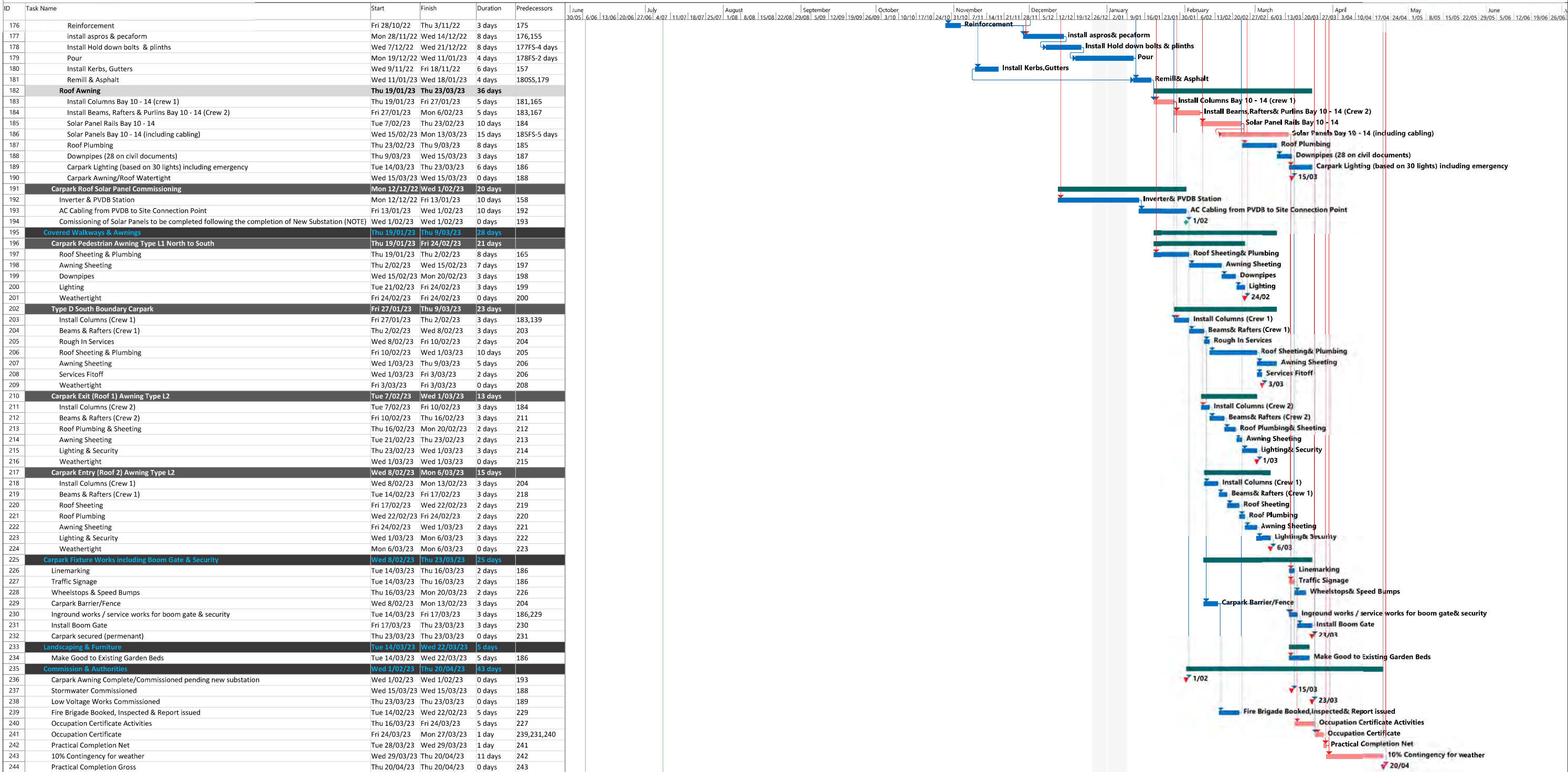






# NEWCASTLE AIRPORT TERMINAL EXPANSION

Enabling Works Programme  
Construction Control Offer Programme - Revision 1





# Annexure B

## Staging Plan

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Rev	Description	By	Date
A	FOR CLIENT REVIEW	COX	08/04/2022

CARPARK SITE LEGEND

- EXISTING BUILDING
- CARPARK SCOPE BOUNDARY (OUT OF SCOPE HALFTONE)
- EXISTING TREES
- FIRE HOSE REEL
- FIRE HYDRANT
- FLOOD LIGHT
- SITE BOUNDARY LINES
- EXISTING SIGN LOCATION
- EXTENT OF TERMINAL DA

ADDITIONAL NOTES

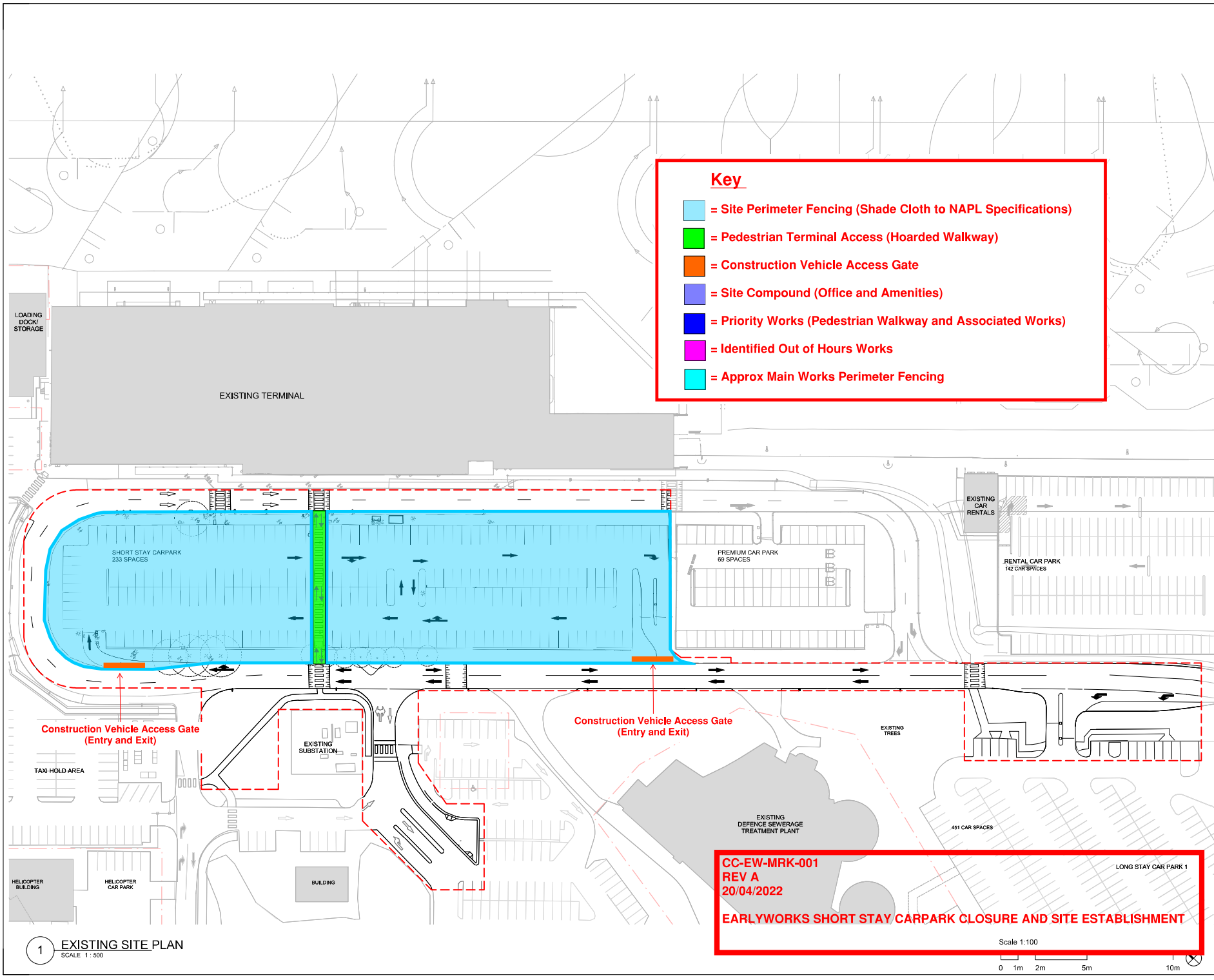
- THIS DRAWING IS TO BE READ IN CONJUNCTION WITH REFER ### FOR LANDSCAPING, ### FOR CIVIL, ### FOR STRUCTURE.

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HATCH LCI CONTEXT  
SCG, Purely Access Pty Ltd, Philip Chun, Ethos Urban, JMT, LTS Lockley, Douglas Partners

Client: Newcastle Airport  
Project No: 221139.00  
Project: NEWCASTLE AIRPORT CAPACITY ENHANCEMENT  
Newcastle Airport Terminal Williamtown Drive, Williamtown NSW 2318, Australia  
Drawing Title: NEW PREMIUM CARPARK - EXISTING SITE PLAN

Document Control Status:  
Co-ordinated: CDG  
Project Architect: KM  
Project Director: DH  
Drawing Number:  
Scale: 1:500 @ A1  
Date: 20/04/17  
Revision:  
NEWCASTLE AIRPORT CAPACITY ENHANCEMENT - 01- AR- DRG-DA-07-01








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**Nominated Architects**  
One Angus no. 6491

\_\_\_\_\_

Project No. \_\_\_\_\_

Project	NEWCASTLE AIRPORT
---------	-------------------

Newcastle Airport Terminal Williamtown Drive,

Williamstown NSW 2316, Australia

NEW PREMIUM CARPARK - SITE

\_\_\_\_\_

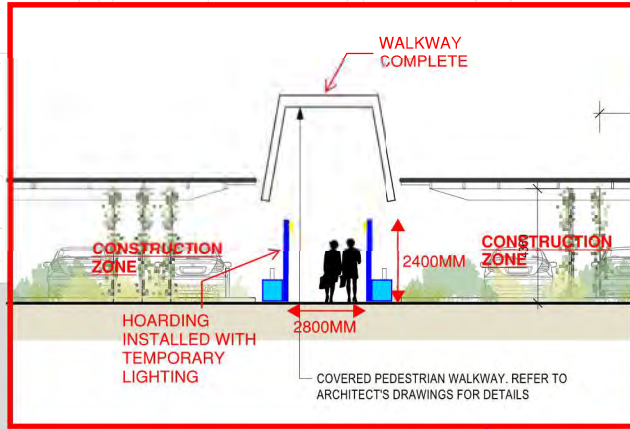
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Project Architect: \_\_\_\_\_ Scale: \_\_\_\_\_

	DH	26/04/17
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ENHANCEMENT- 01- AR-

## SECTION A-A

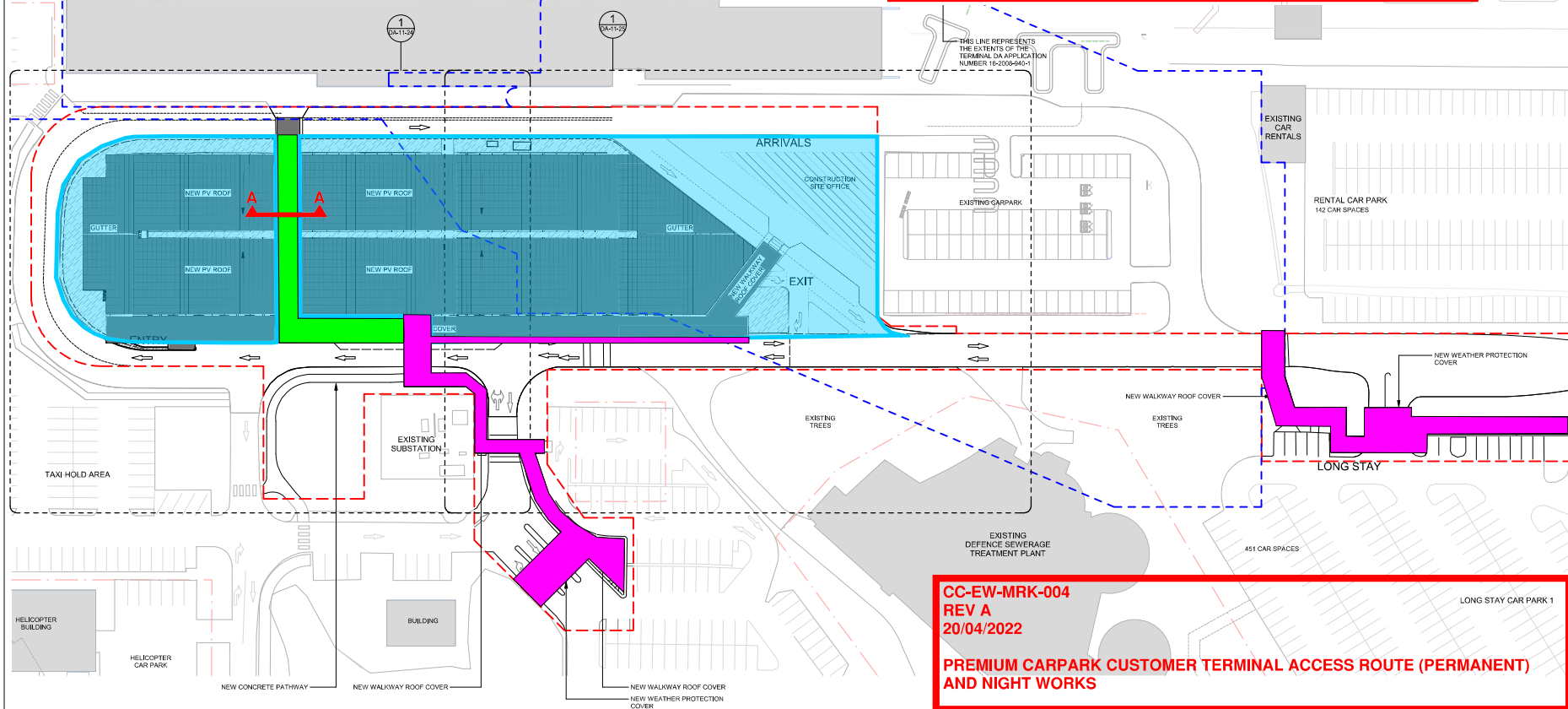


### Key

- = Site Perimeter Fencing (Shade Cloth to NAPL Specifications)
- = Pedestrian Terminal Access (Hoarded Walkway)
- = Construction Vehicle Access Gate
- = Site Compound (Office and Amenities)
- = Priority Works (Pedestrian Walkway and Associated Works)
- = Identified Out of Hours Works
- = Approx Main Works Perimeter Fencing

### CARPARK SITE LEGEND

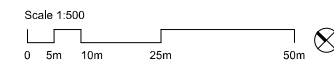
- EXISTING BUILDING
- CARPARK SCOPE BOUNDARY (OUT OF SCOPE HALFTONE)
- EXISTING TREES
- FIRE HOSE REEL
- FIRE HYDRANT
- FLOOD LIGHT
- SITE BOUNDARY LINES
- EXTENT OF TERMINAL DA
- DIRECTION OF ROOF FALL
- PROPOSED ROOF



**CC-EW-MRK-004  
REV A  
20/04/2022**

**PREMIUM CARPARK CUSTOMER TERMINAL ACCESS ROUTE (PERMANENT)  
AND NIGHT WORKS**

**1 PREMIUM CARPARK - PROPOSED SITE PLAN**  
SCALE 1 : 500



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**HATCH** **LCI** **CONTEXT**  
SCG, Purely Access Pty Ltd, Philip Chun, Ethos  
Urban, JMT, LTS Lockley, Douglas Partners

Client: **Newcastle Airport**  
Project No: **221139.00**  
Project: **NEWCASTLE AIRPORT  
CAPACITY ENHANCEMENT  
Newcastle Airport Terminal Williamtown Drive,  
Williamtown NSW 2318, Australia**  
Drawing Title: **NEW PREMIUM CARPARK - SITE  
PLAN**

Document Control Status: **NOT FOR CONSTRUCTION**  
Co-ordinated: **CDG** Drawn: **AF**  
Project Architect: **KM** Scale: **1 : 500 @ A1**  
Project Director: **DH** Date: **20/04/17**  
Drawing Number: **NEWCASTLE AIRPORT CAPACITY  
ENHANCEMENT - 01- AR-  
DRG-DA-09-01** Revision: **A**

Rev	Description	By	Date
A	FOR CLIENT REVIEW	COX	08/04/2022

# CARPARK SITE LEGEND

- EXISTING BUILDING
- CARPARK SCOPE BOUNDARY (OUT OF SCOPE HALFTONE)
- EXISTING TREES
- FIRE HOSE REEL
- FIRE HYDRANT
- FLOOD LIGHT
- SITE BOUNDARY LINES
- FENCE LINE
- LANDSCAPED ZONE
- LINE MARKING
- EXTENT OF ELECTRICAL CABLES FOR EV CHARGERS

# CARPARK COLOUR LEGEND

- DROP OFF ZONE
- PICK UP ZONE
- TAXI PICK UP ZONE
- BUS STOP ZONE

# DROP OFF AND PICK UP ZONES

PICK UP ZONE ADDED: 6 CARS  
PICK UP ZONE REMOVED: 1 CAR

IN SCOPE RETAINED (KERB ADJUSTED)  
DROP OFF ZONE: 10 CARS  
PICK UP ZONE: 3 CARS

Parking Schedule			
Type	Width	Length	Count
Accessible Car	2700	5600	7
Accessible Car - EV	2700	5600	1
Accessible Car - Future EV	2700	5600	1
Standard Car	2700	5600	116
Standard Car - EV	2700	5600	3
Standard Car - Future EV	2700	5600	35
163			

Newcastle Airport

**Newcastle Airport**

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**COX**

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Nominated Architects  
Jinfa Square No. 6811  
Fuzhou, China No. 3362

**HATCH** **LCI** **CONTEXT**

SCG, Purely Access Pty Ltd, Philip Chun, Ethos  
Urban, JMT, LTS Lockley, Douglas Partners

Client: Newcastle Airport  
Project No: 221139.00

Project: NEWCASTLE AIRPORT  
CAPACITY ENHANCEMENT  
Newcastle Airport Terminal Willamtown Drive,  
Willamtown NSW 2318, Australia

Drawing Title: NEW PREMIUM PARKING - FLOOR  
PLAN

Document Control Status: NOT FOR CONSTRUCTION

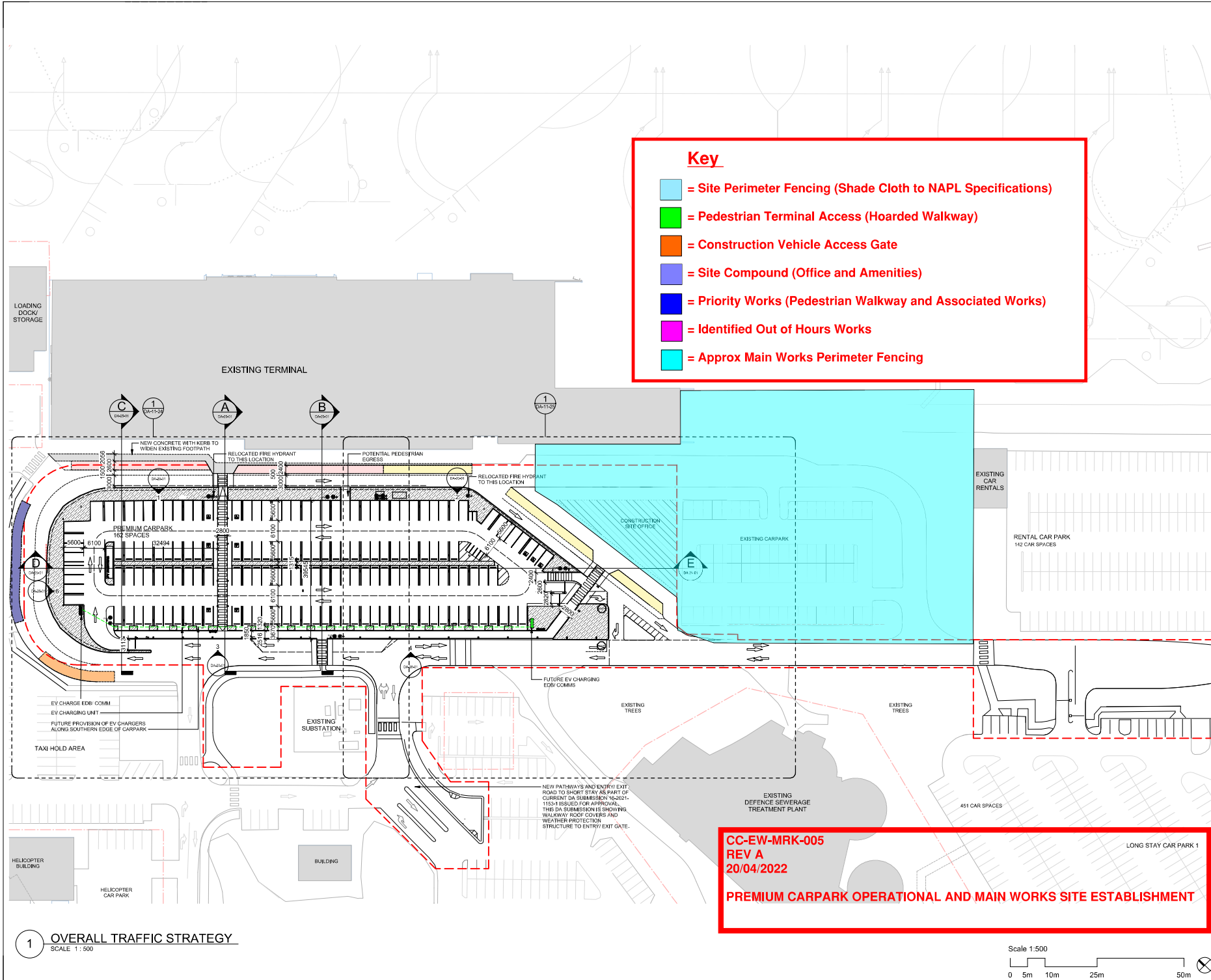
Co-ordinated:	CSD	Drawn:	AF
Project Architect:	KM	Scale:	1: 500 @ A1
Project Director:	DH	Date:	20/04/17
Drawing Number:		Revision:	

NEWCASTLE AIRPORT CAPACITY  
ENHANCEMENT - 01- AR-  
DRG-DA-09-02

ROUTED DATE: 04/05/2022 09:58:04

# Key

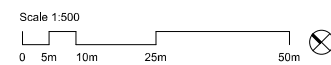
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1 OVERALL TRAFFIC STRATEGY  
SCALE 1: 500

CC-EW-MRK-005  
REV A  
20/04/2022

PREMIUM CARPARK OPERATIONAL AND MAIN WORKS SITE ESTABLISHMENT



# Annexure C

## Traffic Management Plan



